

GLENWOOD SOUTH

STREETSCAPE

&

PARKING PLAN

January 31, 2000

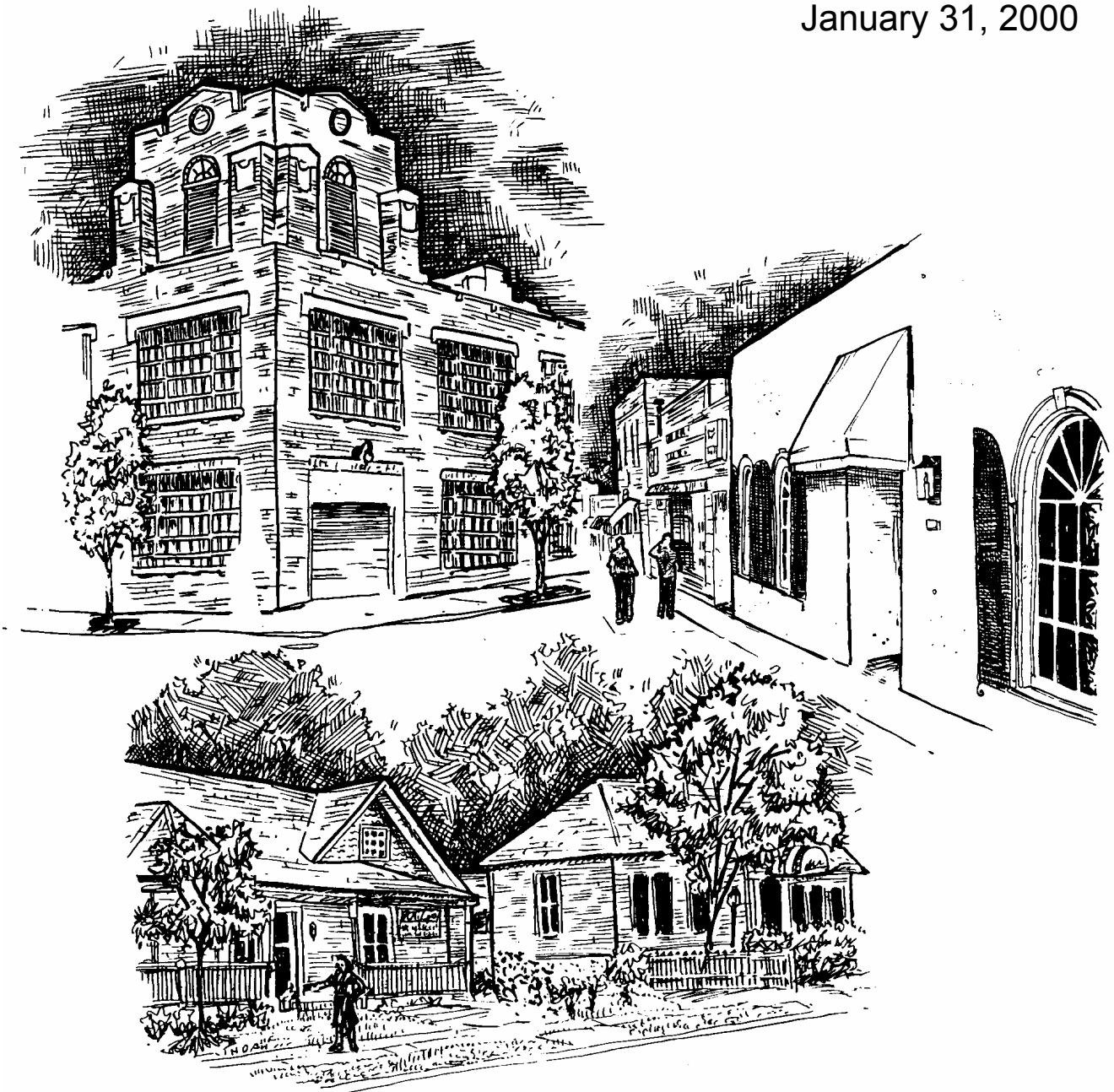


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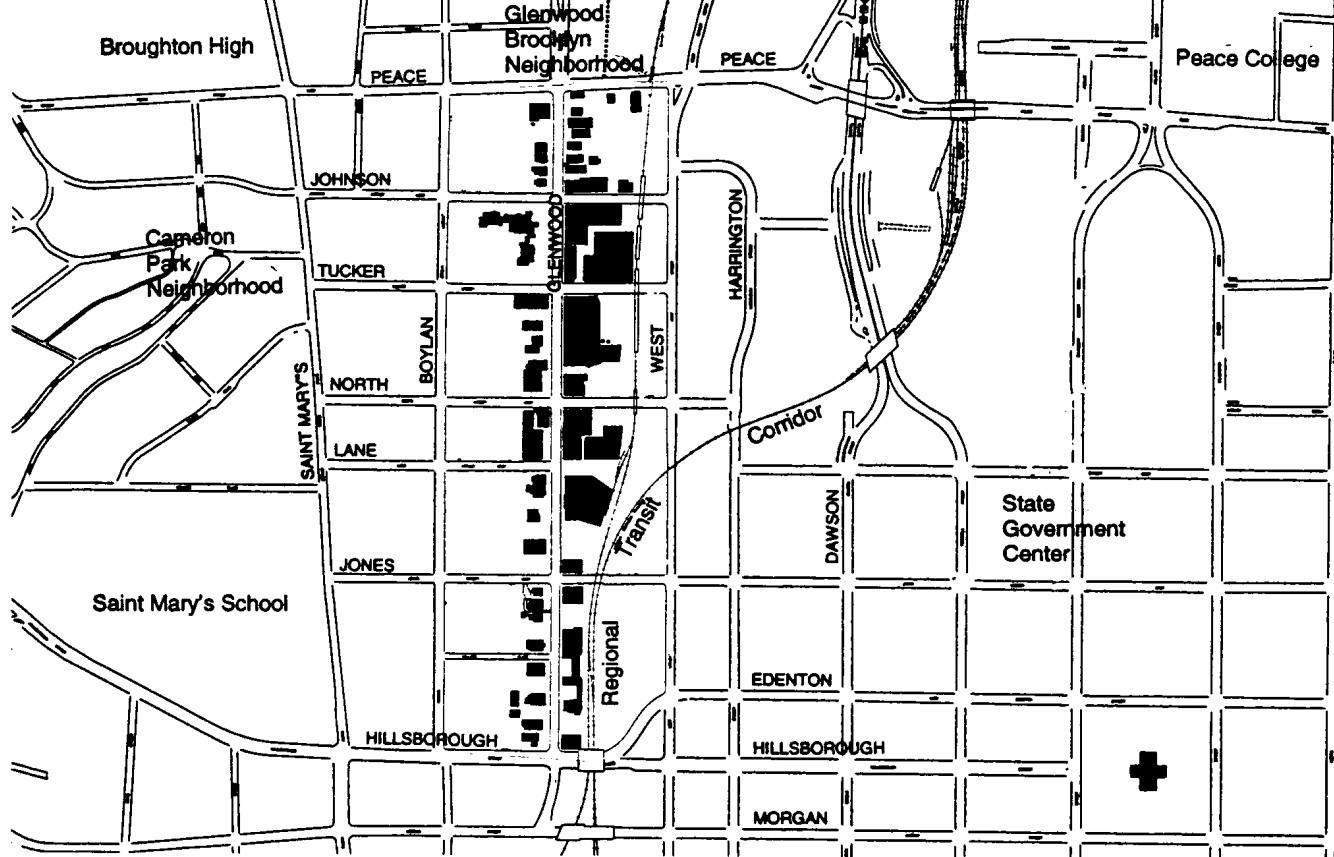
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Introduction

Background

In December of 1997, the City Council adopted the Glenwood South Small Area Plan and authorized staff to prepare a streetscape and parking plan for the Glenwood Avenue commercial area. A participatory planning process involved local business and property owners in the identification of streetscape improvements and the coordination of parking to support business investment.

Planning Goal

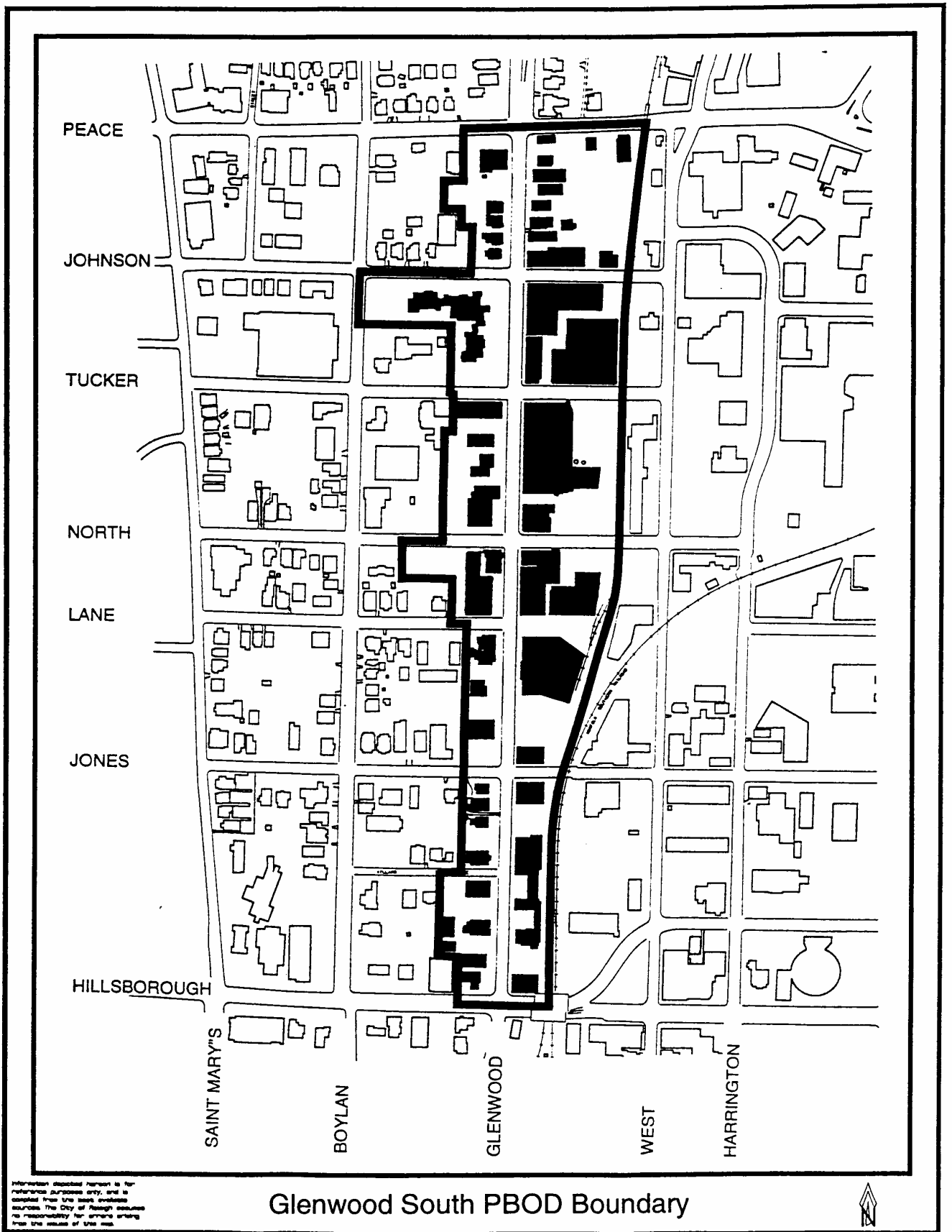
The Glenwood South commercial center is unique to Raleigh and is experiencing renewed development interest contributing significantly to the downtown growth trend. An objective of this plan is to emphasize the unique identity of the area as an urban entertainment and business destination within the downtown region. To support continued development investment in the area and an efficient urban pattern, improvements to the pedestrian environment and connections to adjacent employment centers, transit stops and parking are necessary. The goal of this plan is to create a coordinated streetscape concept, recommend parking improvements and to fine tune code standards necessary to support a pedestrian oriented urban development pattern.

Project Location

The streetscape plan includes properties along Glenwood Avenue from Hillsborough Street north to Peace Street as shown on the project boundary map. The north-south oriented linear plan area includes approximately 20.2 acres and is bounded by a potential downtown growth area and the State Government Center to the east and a low intensity mixed use neighborhood to the west.

Streetscape Concept

Glenwood Avenue is a transitional boundary between the downtown urban area and the low intensity mixed use areas west of the street. The east side of Glenwood is characterized by one to four story commercial style buildings located adjacent to the sidewalk. The west side includes a mix of one story residential buildings and one to two story commercial buildings setback from the sidewalk with landscaped lawns and a five to eight foot planting strip between the curb and sidewalk. The following streetscape standards build upon these characteristics to accent the transition in development intensity while establishing unifying elements for the streetscape.



Streetscape Standards

Sidewalk Paving

East Side Within the twelve to fifteen foot right-of-way strip from the curb to building wall, a coordinated pattern of concrete paving with a scored grid and concrete unit pavers should be used. Several pavement patterns are recommended to allow design interest and flexibility. As a unifying element, a continuous five-foot wide concrete sidewalk should extend along the block face. The concrete sidewalk may be broken with unit pavers only at primary building entrances. A consistent pattern of concrete paving and unit pavers should be used along the entire length of a single block face. Below are several recommended pavement patterns and recommended unit pavers.

West Side The existing five foot sidewalk should be retained with repairs made where needed. In existing urban settings, such as between Lane and North Streets, or when new development is brought

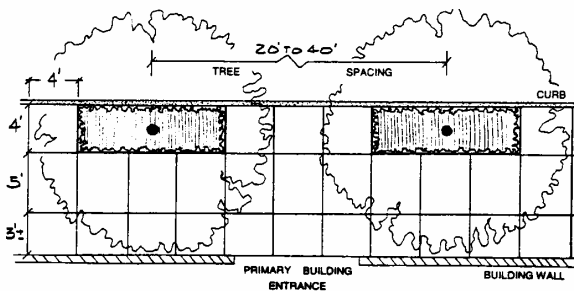
up to the sidewalk; the pavement design recommended for the east side of Glenwood should be used.

On both sides of the street where driveways cross the sidewalk, the concrete sidewalk pavement pattern should be continued across the drive. A ramp type driveway design should be used to prevent interruption of the sidewalk with handicap ramps down to the driveway.

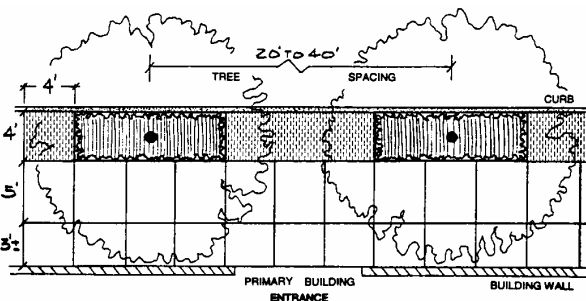
Curbing

Granite curbs are present along the majority of the Glenwood streetscape and should be preserved as redevelopment occurs. Unused curb cuts and areas where the curb has been replaced with concrete should be restored to match existing granite curbs. Existing granite curbs should be excavated and reset when sidewalks are repaired to reestablish a six-inch street curb.

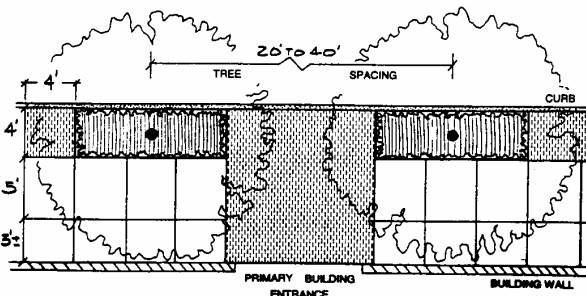
East Side



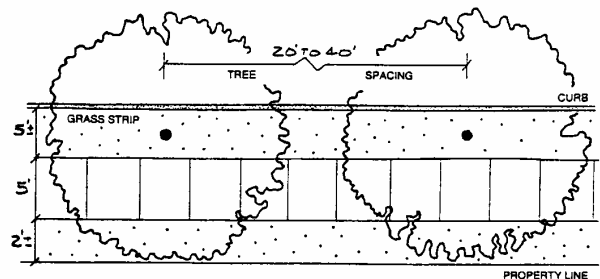
All concrete pavement with scored grid.



Unit Pavers with lateral running bond.



West Side



Concrete pavement with grass strips.

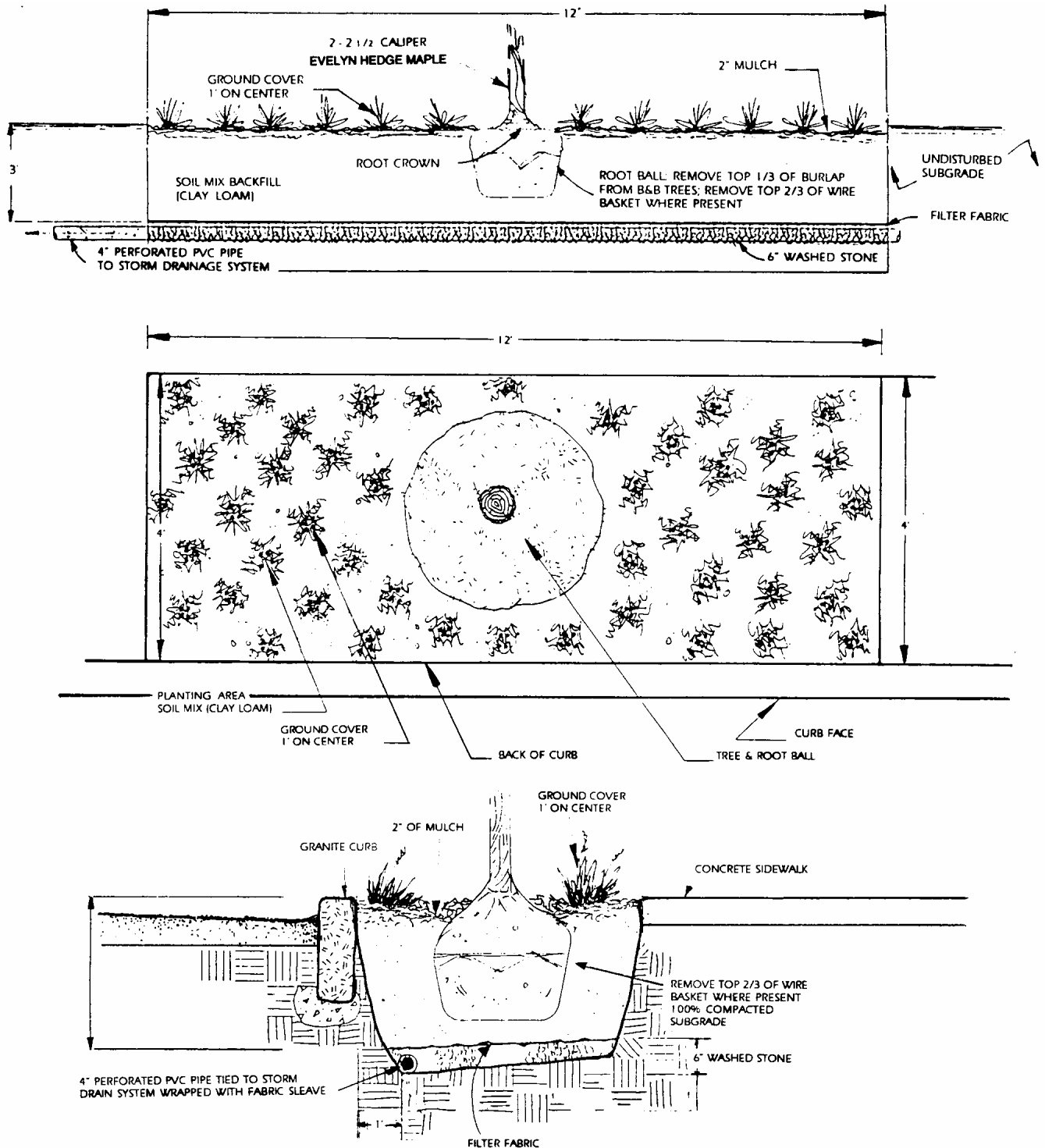
Paver manufacturer and style:
Belgard – Guilford Blend
Gloria Enterprises, PO Box 14489
Greensboro, NC 27415

Street Trees

The recommended street tree for this section of Glenwood Avenue is the 'Evelyn' hedge maple (*Acer campestre* 'Evelyn'). This tree was selected for its ability to adapt to urban environmental conditions and its upright branching habit. The branches are slender and branch profusely to create a fine texture particularly during winter. The tree species reaches a mature height of 30 to 35 feet.

Tree planting size should be 2" to 2.5" caliper and be spaced 20' to 40' on center in 4' by 12' planters as shown below. Tree placement should relate to the

architectural elements of the adjacent buildings and be coordinated with business uses so not to interfere with loading and visibility needs. An open tree planter measuring 4 feet by 12 feet should be used for each tree. The existing soil should be removed from the planter to a depth of 3 feet and replaced with a clay loam soil mix to provide adequate root zone. Groundcovers are encouraged in the planters to soften the street edge and provide site interest. English ivy or liriopse should be used as a base planting with seasonal annual plantings maintained by the property owner.



Building Facade

The front face of each building on the street contributes to the overall streetscape and should relate in scale and provide interest to pedestrians along the sidewalk. Large windows with product displays and open views to inside activity are encouraged. Building entrances should open onto the sidewalk and welcome customers to the business inside. Adaptive reuse of residential structures on the west side of Glenwood should be remodeled to reflect the buildings original character.

New buildings and existing buildings undergoing renovation, as required in 10-2055(e)(2), shall break up blank exterior building walls that exceed 15 linear feet by including any one or more of the following:

- A public doorway made of transparent material.
- A doorway made of opaque materials and recessed at least 3 feet.
- A stairway directly available at street level to the public, but not fire escapes or false stairways.
- A fenestration element in keeping with the surrounding architecture of at least 12 square feet in area and no more than 4 feet above the sidewalk at its lowest point.

Parking decks should include sidewalk level commercial/office space to add street level interest. The deck shall be designed to include a variety of architectural elements that relate to the design of the primary structure or neighborhood.

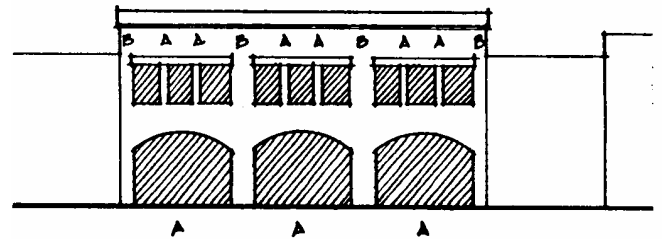
Building Setback

The minimum building setback from a street right-of-way shall be -0- provided the required widths of pedestrian ways are met. Buildings should be set adjacent to the sidewalk on the east side of Glenwood. Existing buildings on the west side typically include a front yard setback. New development should blend with these adjacent building setbacks. Side and rear yard setbacks shall be the same as the underlying zoning district.

Vehicular Parking Areas

No new vehicular parking areas, including additions to parking areas existing prior to the application of a PBOD, shall be located in any portion of the site parallel to and adjoining a thoroughfare. Parking areas may be located behind the front wall of the principal building when a closed decorative wall is built to screen the parking from the sidewalk. The wall shall be 3.5 to 5 feet high, be constructed of an opaque material that complements the primary structure, and include visual articulation with architectural segmentation at least every nine feet.

New Building Facades:



A pattern of window openings, doors, solid walls, and details should be created for each building. This pattern helps reduce the scale of a building and create a pleasing appearance along the street.



Remodeled Building Facades:

Remodel like this.

- Existing wall openings should be maintained.

- Existing building units should not be hidden.



Don't remodel like this.

Building Height

Along the west side of Glenwood Avenue from Willard to North Street and along Peace Street to a depth of 100 feet from the right-of-way, a maximum building height of 40 feet is allowed. Buildings may exceed 40 feet as allowed in 10-2076(a) for Neighborhood Business zoning.

For the remaining lots on the west side of Glenwood Avenue and along the east side north of Johnson Street, a maximum building height of 40 feet is allowed with additional height to a maximum of 80 feet achieved with a one to one stepback ratio above 40 feet.

Along the east side of Glenwood from Hillsborough to Johnson Street, a maximum building height of 60 feet is allowed with additional height to a maximum of 132 feet achieved with a one to one stepback ratio above 60 feet.

Buildings may be constructed to a height greater than 80 feet with the approval of City Council in conformance with the guidelines recommended in Appendix A of the Downtown Design Guide and procedures in 10-2132.

Signage

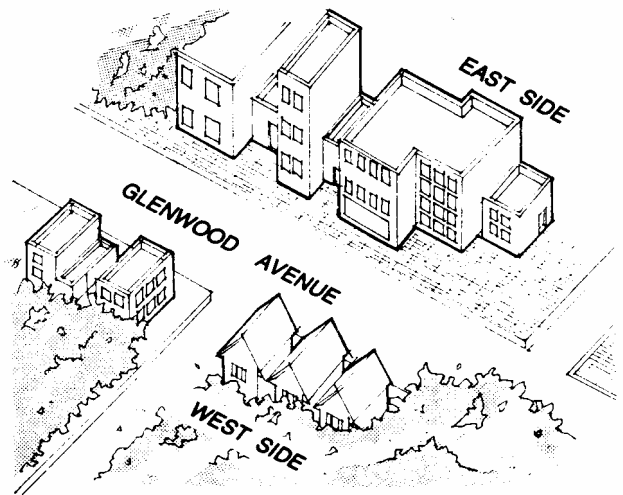
The design and placement of signage should complement the architectural character of the building and be oriented towards the pedestrian. Small high-quality signs are encouraged to enhance the appearance of the building and streetscape.

The following signage types are encouraged: Awning signs, window stencils, wall signs (when internally lit text should be illuminated with opaque background). Low profile ground signs are allowed on private property as allowed by the city code.

The following signage types are prohibited: Projecting signs, internally lit wall signs, back lit signs, off-premise signs, internally illuminated awning signs and internally illuminated signs that do not have an opaque background.

Awnings

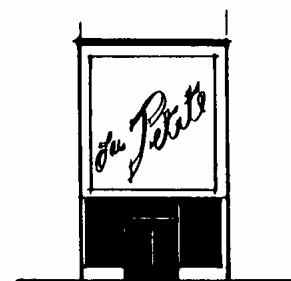
Awnings are encouraged at building entrances and windows to provide cover over the sidewalk, to create an intimate space along the sidewalk for window shopping and to reduce the scale of a building for pedestrians. Awnings should be made of cloth or a cloth-like synthetic and must be at least 7 feet above the sidewalk at their lowest point with any supporting structure located a minimum of 8 feet above the sidewalk. An encroachment agreement approved by City Council is necessary if the awning projects into the public right-of-way.



Acceptable Signs



Unacceptable Signs



Signs that dominate the upper facade.



Signs that overpower the lower facade.

Overhead Utilities

At present, high voltage electrical lines are located on the east side of Glenwood Avenue. The lines are consolidated on wooden poles about 30 feet above the ground and do not greatly dominate the streetscape. Telephone and cablevision lines are located primarily on the west side of Glenwood. These services are located lower on wooden poles, often cross the street and tend to create an unorganized cluttered appearance within the streetscape. A greater number of wooden poles are required to carry these lines than needed for electrical lines on the east side of the street.

The electrical lines on the 400 and 500 blocks of Glenwood will be relocated to the west side of the street with the development of the Creamery and 510 Glenwood projects due to building clearance and safety requirements. Additional relocations to the west side of Glenwood may be necessary as redevelopment occurs on the east side of Glenwood.

The cleanest solution to the electrical line conflicts would be to place them underground on the east side of the street, though a very expensive proposal

at roughly \$1.75 million from Hillsborough to Peace. Raising the height of the lines would be of similar difficulty and expense. The most cost efficient option is to consolidate the lines and reduce the number of poles where possible. The incremental relocation of lines, as redevelopment requires, to the west side of Glenwood should be placed parallel to and at a consistent distance from the street. Lines crossing the street should be minimized as well as the number of supporting poles.

Recommendation:

Underground the cable and telephone service lines using a directional bore under the sidewalk on the west side of Glenwood. Traffic signal and street light lines should be relocated underground on the east side of Glenwood. As redevelopment occurs on the east side, a 3 inch conduit for traffic signal lines and a 2 inch conduit for street light lines should be placed 3 feet under the sidewalk. Black steel poles with mastarms are recommended at signalized intersections to help reduce overhead clutter and organize lighting and pedestrian crossing signals.

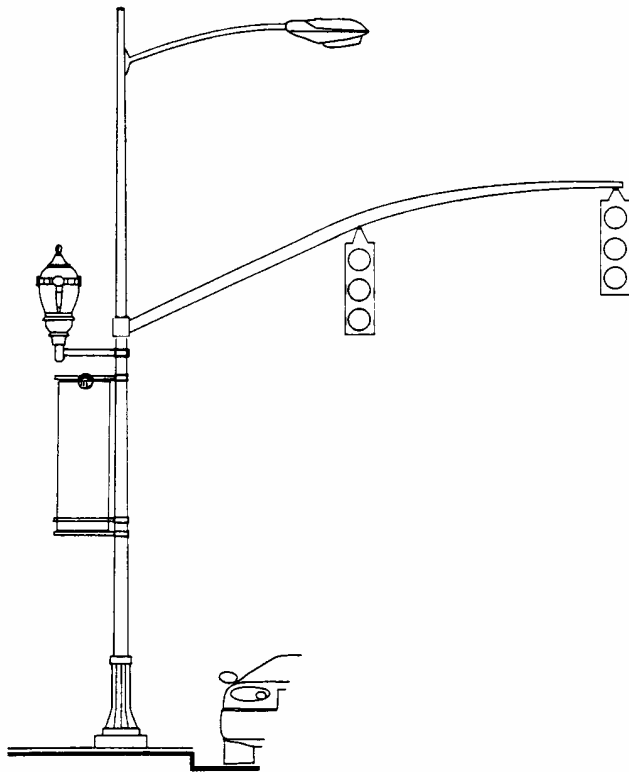


Street Lights

Adequate lighting of the sidewalk and street area is essential to creating a safe and inviting streetscape. A combination of pedestrian scaled light fixtures and standard black cobra head street light fixtures on black steel poles are recommended to assure a well lit street area and to establish a unifying element along the street. The pedestrian scaled fixtures will improve lower level sidewalk lighting and the higher cobra head lights fill in the street and intersection areas.

Street light poles should be placed between tree plantings to prevent shading and maximize lighting. At signalized street intersections, a black steel pole and mastarm should be used for traffic and pedestrian signals to complete the utility package. A consistent lighting style is encouraged for on-site improvements within 50 feet of the right-of-way.

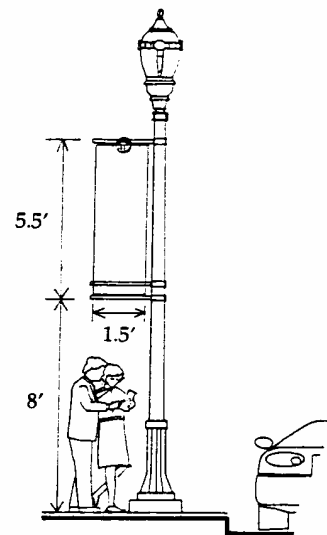
Business owners are encouraged to further brighten the sidewalk and accent their business location by leaving display window and interior lighting on at night. On-site lighting should be designed in such a way as to prevent the direct view of the light source from neighboring residential areas.



Street Intersection Light and Signal Pole: Black galvanized steel pole with traffic signal mastarm and black cobra head light fixture.

Banners

Banners at street intersections are recommended to identify the Glenwood South District and special area wide events along the corridor. Advertisement or listing of individual businesses on the banners would not be allowed. The banners should be attached to a utility pole above the pedestrian light over the sidewalk. The banner dimension should not exceed a maximum width of 1.5 feet and maximum height of 5.5 feet and have 8 feet of clearance above the sidewalk. Funding for design, creation and installation of the banners would be possible through an established merchants association. Final design and encroachment agreements must be approved through the City Council.



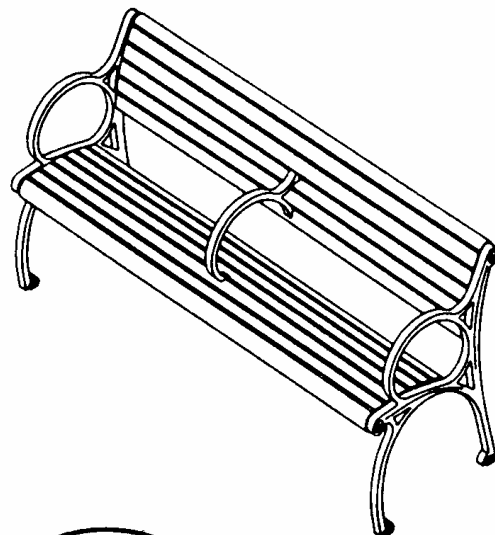
Mid-block Pedestrian Light: Black galvanized steel pole with Masterpiece Series “Whitman” luminaire style available through CP&L.

Street Furniture

Street furnishings provide an important level of detail that improves the comfort and convenience of the streetscape for pedestrians and business patrons. Facilities to accommodate transit use and to enhance personal safety are also important to the quality of the streetscape. Use of a consistent street furniture style is recommended on-site within 50 feet of the right-of-way to maintain a consistent streetscape treatment.

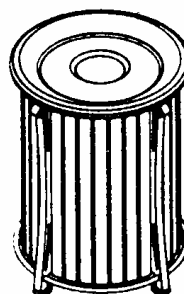
Transit Stop Bench:

Plainwell with center arm,
Ipe wood/Grotto powdercoat
Landscapeforms
431 Lawndale Avenue
Kalamazoo, Mi. 49001



Litter Receptacle:

Plainwell 35 gallon
Ipe wood/Grotto powdercoat
Landscapeforms
431 Lawndale Avenue
Kalamazoo, Mi. 49001



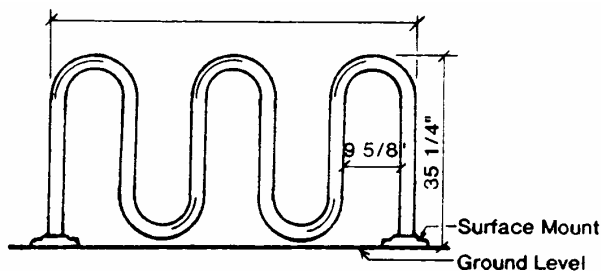
Recycling facilities should be included in the streetscape at locations convenient for use as pedestrian activity increases and the need warrants.

Businesses are encouraged to share dumpster facilities rather than placing individual receptacles on the street for pickup. Dumpsters should be located in the least obtrusive area and be screened from the public right-of-way.

Bike Rack:

Model No. RB 07
Brandir International, Inc.
200 Park Avenue, Suite 303E
New York, NY 10166 (212) 505-6500

Bicycle parking spaces shall be provided for all new buildings and for existing buildings undergoing renovation, as required in 10-2055(e)(2), at a rate of at least one bicycle space for every 20 required automobile spaces. Bicycle parking shall be located within easy access from the street.



Streetscape Implementation

The streetscape recommendations included in this plan are conceptual in nature until specific actions are taken to assure implementation. A coordinated effort by the city and area business/property owners will be necessary to achieve the discussed objectives. Listed below are necessary actions:

- Rezone the area recommended on page 2 of the plan to Pedestrian Business Overlay District. This will adopt as code the recommended standards relating to building placement and design, streetscape design and implementation, and parking standards.
- Allocate funding in the Capital Improvements Program for consultant services to prepare detailed construction plans to identify, in coordination with business/property owners, the placement of street trees, furniture, lighting, and sidewalk paving. The construction plans should also include cost estimates for installation of streetscape improvements.
- Allocate funding to place the overhead service lines underground for telephone, traffic signal and street lights as well as to make intersection improvements to include traffic signal mastarms and pedestrian signals.
- As properties redevelop along Glenwood, improvements to the streetscape are required as specified in the streetscape plan (10-2055(e)(9)). These improvements will include sidewalk repairs, street tree plantings and participation in lighting improvements.

Streetscape Maintenance

A standard maintenance policy has been established for the public right-of-way area between the street curb and the private building frontage on the street. Property owners are responsible for maintaining along their street frontage all sidewalks, street trees, and building fixtures that overhang the sidewalk such as awnings or light fixtures. Maintenance of any vegetative plantings in tree planter areas or in above sidewalk planters shall also be the responsibility of the property or business owner. Maintenance or replacement of street trees must be reviewed by the City Arborist.

Parking Strategy

The development pattern of the Glenwood corridor was established in an era with less dependence on auto travel. Access was oriented primarily to pedestrians who used the trolley system for longer trips between residential and business areas. On-site parking was not typically provided and street parking accommodated the few vehicles used.

In today's environment, parking has become a necessary component of development feasibility and business success due partly to the lack of convenient access options. The high rate of redevelopment occurring along the corridor has made parking convenience a major concern for business owners and their customers, especially at the noon peak when both office and commercial uses are open. While a large amount of parking is available in the area much of it is not efficiently used, shared or conveniently located.

The changing development character of the corridor to an intense mix of office, retail, entertainment and residential uses presents an opportunity to increase access options and reduce the current levels of required parking. The increase in development intensity and the mix of uses along the corridor will create a more convenient pedestrian environment where visitors, residents and employees can meet their daily needs without depending on a personal vehicle for multiple trips. Improvements to the streetscape to create a pleasant and convenient pedestrian environment and the availability of transit options are vital to serve this development pattern.

The following recommendations are intended to improve access to the Glenwood South area through various modes of arrival (pedestrian, automobile, and transit) and to maximize the efficient use of parking and to preserve on-street parking in the area for customer use.

Implemented through application of the Pedestrian Business Overlay District:

- Allow a 15% reduction the required off-street parking for commercial uses for those properties

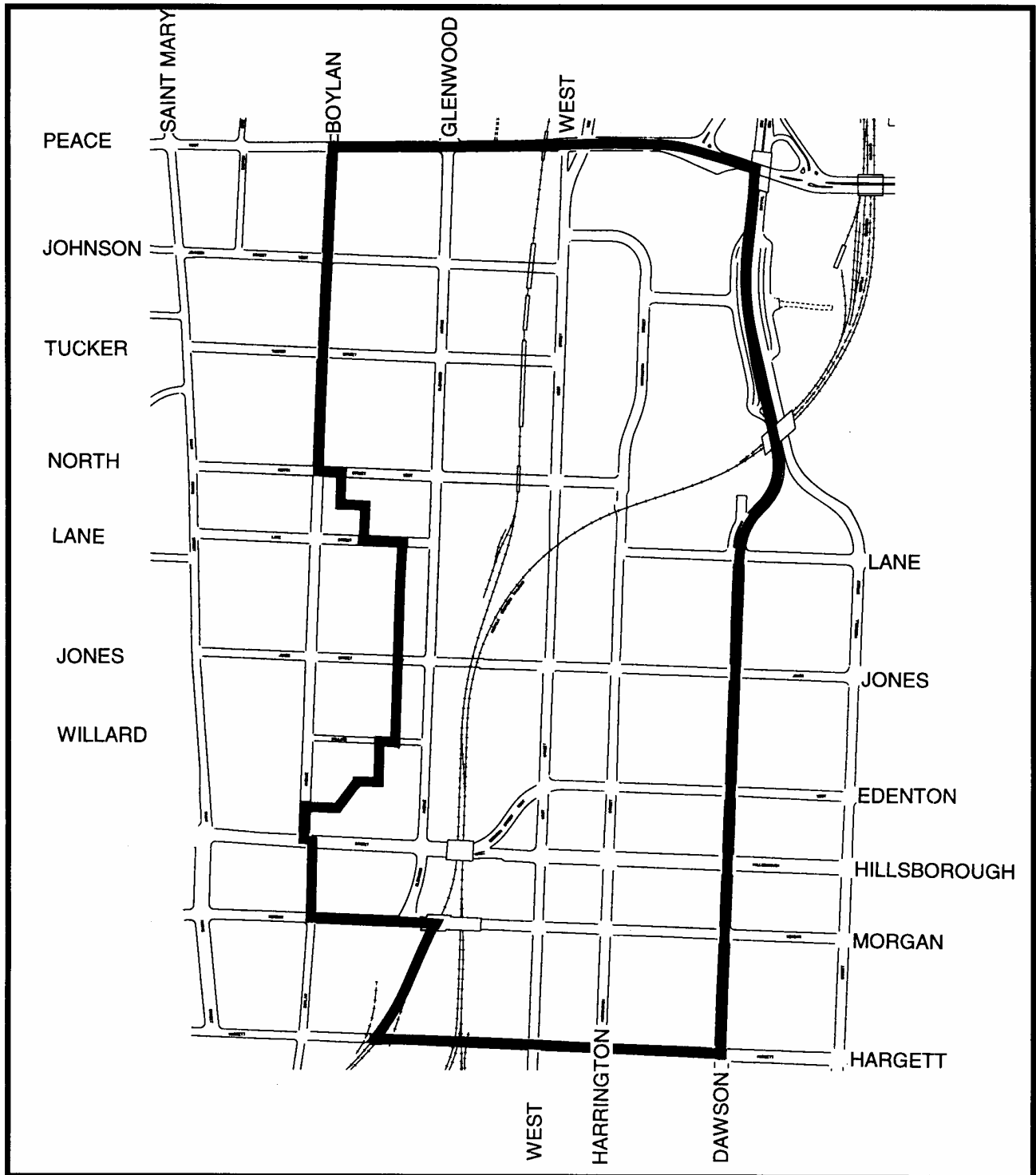
that implement the streetscape standards of this plan.

- Allow a 45% parking reduction for the first 2500 square feet of a single commercial building.
- Total parking reductions may not exceed 45% for a single commercial/recreational use. The developer shall determine the allocation of parking credits to business establishments in a mixed-use building.
- Increase the distance for off-site customer and employee parking to 1200 feet (walking distance) only within the designated off-site parking zone. The off-site parking must be available for public use and not to a specific business.

- Consider a Code amendment to waive transitional protective yards if a business is cooperating in cross access/parking arrangements with other businesses.

Initiated by City of Raleigh

- Extend sidewalks through an assessment program from the Glenwood corridor to the West Street area to improve pedestrian access to underutilized parking areas. Pedestrian connections from this parking area to Glenwood Avenue are few, unpleasant and not adequately lighted.
- Extend the downtown trolley evening service and include a noon service to the Glenwood South corridor. Front door service is recommended to maximize convenience and to support ridership.
- Establish a two-hour maximum on-street parking limit where parking is unrestricted along Glenwood Avenue and intersecting streets to the east.
- Install parking meters in areas needing a quick turnover of customer parking. A one-half hour to one-hour time increment is recommended.
- Conduct as soon as possible a comprehensive, area-wide on-street parking analysis to identify unneeded restricted parking and return these areas to the available parking inventory. Many uses have changed in the area and the associated parking restrictions are no longer needed but remain because a change has not been requested.
- Consider a restricted evening program for non-resident parking in the North Boylan NCOD area.
- Prepare a Code amendment that would allow valet service as an alternative to meeting required parking and prepare a text change for advertisement.
- Explore developing a parking management program for the business area.
- Consider Code amendments to revise the method of calculating required parking for mixed-use development that would consider the increased efficiency of shared parking between the multiple uses resulting in a reduction in required parking.



City of Raleigh

GLENWOOD SOUTH

Off-Street Parking Zone



Scale: 1"=600'

Name: STANKUSM

Date: 12/04/98

Information depicted herein is for reference purposes only, and is compiled from the best available sources. The City of Raleigh assumes no responsibility for errors arising from the misuse of this map.